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Module 5: Public Right-of-Way Accessibility Guidelines (PROWAG) Training



Presenter #1

- Travis Helmkamp, PE, ADAC, Project Manager at Oates Associates
- 12 years of experience assisting municipalities with self-evaluations and transition plans
- Completed plans for communities from 5,000 to 300,000 residents



Presenter #2

- Jim de Jong, ADAC
- Former Director of Great Plains ADA Center
- Certified by the Department of Justice (DOJ) to provide Title II trainings
- Creator of the National ADA Symposium and the ADA Coordinator Training Certification Program



Presenter #3

- Nick de Jong, ADAC
- Completed ADA Coordinator Training Program
- Leads Oates Associates data assessment teams
- Assessed 400 miles of sidewalk in the last five years





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Presentation Goals



Goals for Presentation

- Understanding the history of accessibility rule-making
- Application of federal design standards
- Comparing federal vs. state vs. local code



Goals for Presentation (cont'd)

- Technical requirements of PROWAG:
 - Pedestrian Access Routes (R302)
 - Alternative Pedestrian Routes (R303)
 - On-Street Parking Spaces (R310 & R311)





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History of Accessibility Standards



History of Disability Rights

Pre-Americans with Disabilities Act

MODULE

5

- Section 504 of the 1973 Rehabilitation Act
 - Viewed people with disabilities as a minority group
 - Banned discrimination by recipients with federal funds
- Civil Rights Restoration Act
- Americans with Disabilities Act

6 THE FREE LANCE-STAR, Fredericksburg, Virginia, Tuesday, March 13, 1987

Disabled protesters make stand

Demonstrators crawl up Capitol steps

By WILLIAM M. WELCH
Associated Press Writer

WASHINGTON (AP)—Using their arms or whatever movement they could, dozens of people crawled out of their wheelchairs and up the steps of the U.S. Capitol to dramatize their demand for rights for people with disabilities.

"I'll take all night if I have to," said the youngest, 5-year-old Jennifer Keelan of Denver, as she pulled her small body up the steps.

"Come on Jenny, you're almost there," said Michael Winter of Berkeley, Calif., who was making his own difficult journey up the 83 stone steps of the Capitol's West Front.

They were among 60 or so people who put on the demonstration Monday following a rally at the base of the Capitol steps by about 1,000 people supporting legislation to extend rights to people with disabilities.

"We're not asking for any favors," said I. King Jordan, president of Gallaudet University and the first deaf person to hold that position at the school for people with impaired hearing. "We're simply asking the same rights and equality any other American has."

The focus of the protest was the Americans with Disabilities Act, which passed the Senate last year but has bogged down in the House, despite widespread predictions of its ultimate passage.

The measure would outlaw discrimination based on physical or mental disability in employment, access to buildings, use of the telephone system, use of public and private transportation and in other uses. The Capitol building has ramps for wheelchair access to two of its entrances and ramps and elevators inside to enable people confined to wheelchairs to get around.

"What we did for civil rights in the '60s, we forgot to do for people with disabilities," said Rep. Patricia Schroeder, D-Colo.

Although the bill is supported by the Bush administration and congressional leaders, some have begun questioning the administration's commitment in recent weeks. White House spokesman Marlin Fitzwater denied the support was slipping and said the administration was negotiating with key members of Congress.

"We do support the legislation," Fitzwater said. "We're very supportive of their rights and their cause. President Bush has spoken out on that in the past."

Jennifer Keelan, a second-grader, began crawling up the Capitol steps as soon as the speeches ended.

Her mother, Cynthia Keelan, said Jennifer suffers from cerebral palsy and the girl decided to crawl the steps herself after joining the group Americans Disabled for Accessible Public Transit, or ADAPT.

A friend of hers in that group, 5-year-old Katelyn Perkins of Denver, died in January. "I'm doing it for Kenny," Jennifer said as she reached the top.

"I'm proud of her," her mother said. "That was hard work."



AP Wirephoto

A group of handicapped people crawl up the U.S. Capitol steps Monday to draw support for a bill that would extend civil rights to disabled persons.

Federal Standards and Guidelines Americans with Disabilities Act

MODULE

5

- [Americans with Disabilities Act \(ADA\)](#)
 - Department of Justice (DOJ) adopted on July 26th, 1991
 - Establishes scoping and technical requirements for Title II entities
 - Title II entities include local municipalities



Federal Standards and Guidelines

2010 ADA Standards

MODULE

3

- [2010 ADA Standards for Accessible Design \(2010 ADA\)](#)
 - DOJ adopted on March 15, 2012
 - Minimum scoping and technical design requirements under the ADA of 1990
 - Areas Covered:
 - Accessible routes, site and building elements, plumbing elements, communication, built-in elements, recreational facilities



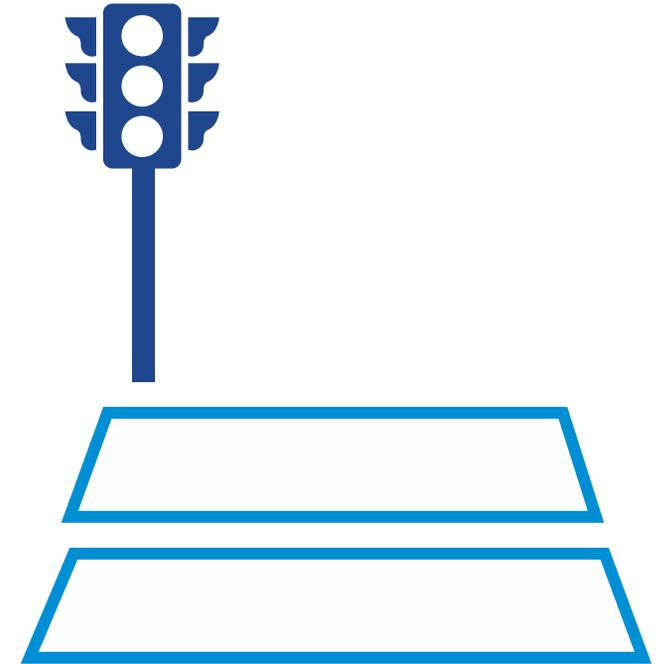
Americans with Disabilities Act US Access Board statement

- “Sidewalks, street crossings, and other elements in the public right-of-way can pose challenges to accessibility. The Board’s ADA and ABA Accessibility Guidelines focus mainly on facilities on sites. While they address certain features common to public sidewalks - such as curb ramps, further guidance is necessary to address conditions and constraints unique to public rights-of-way.”

Federal Standards and Guidelines

PROWAG

- [Public Rights-of-Way Accessibility Guidelines \(PROWAG\)](#)
 - Public Rights-of-Way Advisory Committee, formed in 1999
 - Draft guidelines were released in June 2002
 - Revised draft guidelines were released in November 2025
 - Proposed draft guidelines were released in February 2023



Federal Standards and Guidelines

PROWAG (cont'd)

- U.S. Access Board issued their “Final Rule” on [Public Rights of Way Accessibility](#) August 8, 2023
 - Includes guidelines on how to make the pedestrian facilities - such as sidewalks, crosswalks, shared use paths, and on-street parking, accessible to people with disabilities
- Applies to entities covered under Title II of the ADA, as well as Section 504 of the Rehabilitation Act.
 - Applies to entities covered under the Architectural Barriers Act

Federal Standards and Guidelines

PROWAG (current status)

MODULE

5

- The Access Board issued a document that outlines the changes to PROWAG from previous versions
- This document is available on their [website](#)

U.S. Access Board
Advancing Full Access and Inclusion for All

En Español

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Notable Changes in Public Right-of-Way Accessibility Guidelines Final Rule

September 01, 2023

In 2011, the U.S. Access Board published [proposed accessibility guidelines](#) for pedestrian facilities in the public right-of-way and revised them in 2013 to include shared use paths. The notice of proposed rulemaking (NPRM) on these guidelines for pedestrian facilities and the supplemental notice of proposed rulemaking (SNPRM) on shared use paths requested public comments on all provisions of the proposed accessibility guidelines. More specifically, the Board sought comments from regulated entities, including state and local governments, on the costs and impacts of certain provisions of the proposed rule. The Board carefully reviewed over 600 public comments received in response to the NPRM and SNPRM before issuing the [final rule on accessibility guidelines for pedestrian facilities in the public right-of-way](#) on August 8, 2023.

Listed below are some of the notable changes to the final rule from the versions proposed in the NPRM and SNPRM. These and other changes are described in further detail in the [preamble to the final rule](#).

Advisory Language. In the final rule, the Board eliminated the advisories as these are not mandatory requirements. The Board will provide guidance on its website to assist regulated parties with understanding and properly implement the final enforceable standards that are issued by the standard-setting agencies. Some information that previously appeared in an advisory was moved to the final rule and is thus mandatory. For example, the final rule requires pedestrian circulation paths at driveways controlled with yield or stop control devices or traffic signals to have detectable warning surfaces. In the NPRM, this requirement was discussed in advisory [R208.1](#).

Manual on Uniform Traffic Control Devices for Streets and Highways (MUTCD). In the final rule, MUTCD provisions are not incorporated by reference. Instead, the Board has stated all required technical provisions along with applicable definitions directly in the final rule. This approach differs from the NPRM where the Board proposed to incorporate by reference various sections of the MUTCD.

Alterations. In the final rule, the Board has modified the definition of "alteration" and provided a definition for "developed." These revisions allow added facilities to comply to the maximum extent feasible where existing physical constraints make compliance

Featured News



[Notable Changes in Public Right-of-Way Accessibility Guidelines Final Rule](#)



[U.S. Access Board Issues Final Rule on Public Right-of-Way Accessibility Guidelines](#)

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2010 ADA Standards vs. PROWAG

Pedestrian Access Route

2010 ADA Standards

- Pedestrian Access Route (PAR)
 - 3 feet with temporary reductions to 32 inches for 2 feet



PROWAG

- Pedestrian Access Route (PAR)
 - 4 feet minimum



2010 ADA Standards vs. PROWAG Sidewalk Grades

2010 ADA Standards

- When sidewalk grade is greater than 5%, they are considered ramps and require handrails and 30" maximum rise

PROWAG

- Sidewalk grade can match grade of roadway



2010 ADA Standards vs. PROWAG

Curb Ramps

2010 ADA Standards

- No maximum length of curb ramp

PROWAG

- Where the curb ramp length must exceed 15 feet to achieve a 1:12 (8.3%) running slope, the curb ramp length shall extend at least 15 feet and may have a running slope greater than 1:12 (8.3%) (R304.2.1)

2010 ADA Standards vs. PROWAG Intersection Grades

2010 ADA Standards

- 2% maximum cross slope

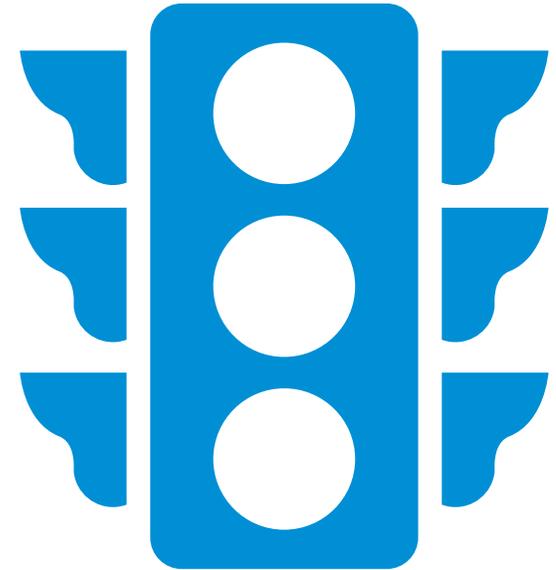
PROWAG

- 5% maximum cross slope at yield or non-stop controlled crossings
- Match road grade at mid-block crossings

Federal Standards and Guidelines

MUTCD

- [MUTCD 11th Edition - FHWA MUTCD \(dot.gov\)](https://www.fhwa.dot.gov/mutcd/11thedition/)
- Published December 2023
- Issued by Federal Highway Administration (FHWA)
- APS standards are incorporated directly into the final PROWAG guidelines



Why follow a guideline?

- Represents the FHWA best practices for accessibility issues:
 - [Link to more information](#)
- Recommended for Local Public Agencies (LPAs) by IDOT
 - BLR Section 41-6.01
- Several changes from 2010 ADA standards to assist LPAs in providing accessibility
 - Commit to one set of requirements





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Illinois Accessibility Standards



State Standards and Guidelines

Illinois Accessibility Code

MODULE

3

- [Illinois Accessibility Code \(IAC\)](#)
 - Minimum scoping and technical design requirements in the State of Illinois
 - Modeled on the 2010 ADA Standards
 - Holds several key differences
 - Areas Covered:
 - Public facilities, multi-family housing



Capital Development Board
2018 Illinois Accessibility Code
Effective October 23, 2018



State Standards and Guidelines

IDOT Standards

- Illinois Department of Transportation (IDOT)
 - [Chapter 48-2.04 of the Design and Environment Manual](#)
 - State highway network and interstates
 - [Chapter 41-6 of the Bureau of Local Roads](#)
 - Local streets including functionally classified roadways
 - [Highway Standards](#)
 - [District Design Standards](#) (District 1)
 - [District Design Standards](#) (District 3)



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Local Accessibility Standards



Applying Design Standards

Local Standards

MODULE

5

- Adoption of municipal standards
 - Establish minimum standards for all development
 - PROWAG on public rights-of-way
 - [Accessible Electric Vehicle Charging Stations](#)
 - Develop additional standards to meet City goals
 - Example: Number of ADA parking spaces





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Comparing Design Standards

Applying appropriate design standards



Applying Design Standards Excerpt from ADA

- Americans with Disabilities Act
 - Technical Requirements –
 1. To ensure compliance with all codes
 2. Know your state and local codes
 3. Use the more stringent technical requirement - *the requirement that provides greater access for individuals with disabilities*

Applying Design Standards

Excerpt from Illinois Accessibility Code

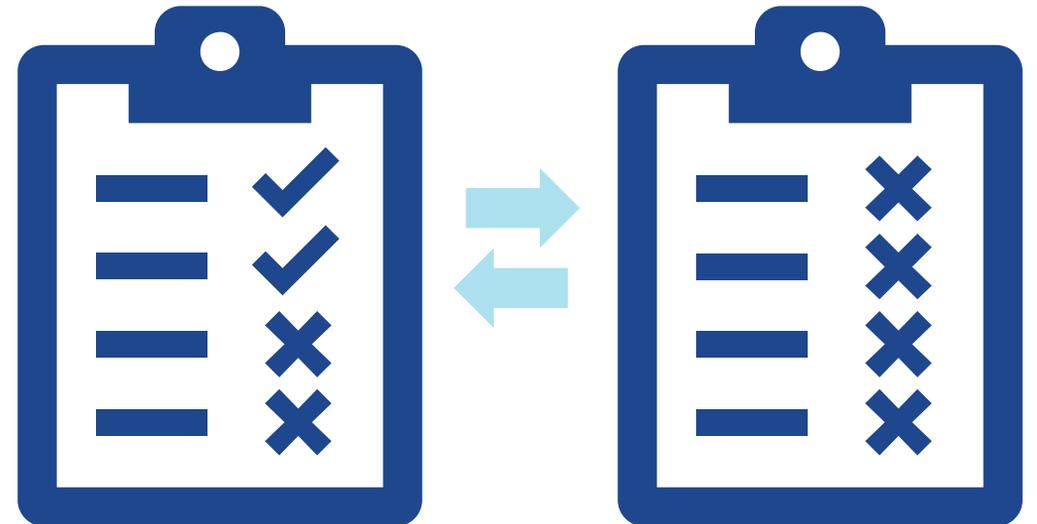
- Illinois Accessibility Code is the umbrella standard for all construction in the state
- Illinois Accessibility Code:
 - *101.7 – Pursuant to Section of the, Environmental Barriers Act, and governmental unit may enact more stringent requirements to increase and facilitate access to the built environment by individuals with disabilities*

Applying Design Standards Protection for Local Agencies

- Include a statement such as:
 - “...that the applicable federal, state, or local standards or guidelines that provide the highest level of accessibility shall govern...”
- Protects in instances where federal or state standards are enhanced

Federal vs. State vs. Local

- How do we compare all these standards?



Questions?



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15-Minute Break #1





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Key Differences Between PROWAG and IDOT



PROWAG vs. IDOT Standards

Sidewalk Width

IDOT Standards

- A sidewalk grade greater than 5% is considered a ramp and requires handrails and 30” maximum rise

Widths. A typical sidewalk is 5 ft (1.5 m) with a 2 ft to 3 ft (600 mm to 1.0 m) wide buffer area between the curb and sidewalk. If no buffer area is provided, the sidewalk should be 6 ft (1.8 m) wide to accommodate any appurtenances that may be included in the sidewalk. High pedestrian volumes may warrant greater widths in business areas and school zones. The minimum sidewalk width is 4 ft (1.2 m). However, this width must be evaluated against the requirements of ADA / PROWAG presented in [Section 41-6](#).

[Cross Section Elements \(illinois.gov\)](#) – Chapter 31-2.02 (2)

PROWAG

- Sidewalk grade can match grade of roadway

R302.2 Continuous Clear Width

Except as provided in R302.2.1 and R302.2.2, the continuous clear width of *pedestrian access routes* shall be 48 inches (1220 mm) minimum, exclusive of the width of any curb.

[R3: Technical Requirements \(access-board.gov\)](#) – R302.2

PROWAG vs. IDOT Standards

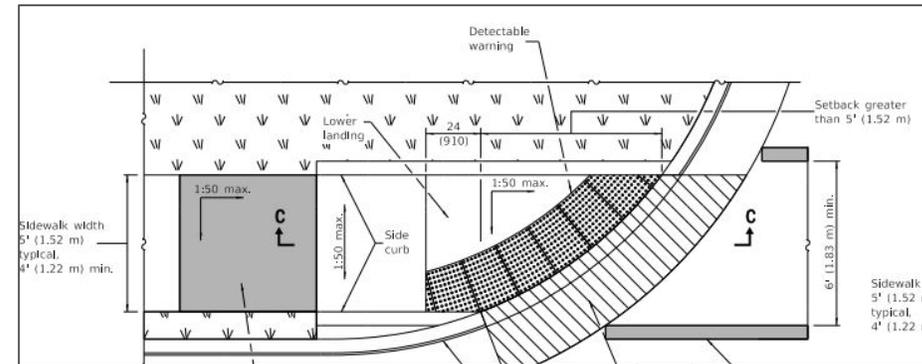
Top Landing at Curb Ramps

IDOT Standards

- “Level” landing is required at the top landing of curb ramps

PROWAG

- Where a change in direction is necessary to access a curb ramp from a pedestrian access route, a landing shall be provided at the top of the curb ramp



[226-424001-11 PerpendicularCurbRamps.pdf \(illinois.gov\)](#)

R304.2.5 Landing

When a change in direction is necessary to access a *curb ramp* from a *pedestrian access route*, a landing shall be provided at the top of the *curb ramp*. The landing shall be 48 inches (1220 mm) wide minimum by 48 inches (1220 mm) long minimum. At *shared use paths*, the landing shall be as wide as the *shared use path*. Where a landing serves only

[R3: Technical Requirements \(access-board.gov\)](#) – R304.2.5

PROWAG vs. IDOT Standards

Alternate Routes

IDOT Standards

- When a pedestrian circulation path is temporarily closed, an alternate PAR shall be marked and/or provided

PROWAG

- Signs identifying alternate pedestrian access **shall be provided in advance of decision points**

41-6.06 Alternate Pedestrian Access Route

When a pedestrian circulation path is temporarily closed by construction, alterations, maintenance operations, or other conditions, an alternate PAR shall be marked and/or provided. Any pedestrian barricades and channelizing devices shall comply with the *ILMUTCD*.

[Microsoft Word - Document2 \(illinois.gov\)](#) – Chapter 41-6.06

R303.2 Signs

Signs identifying alternate *pedestrian access routes* shall be provided in advance of decision points and shall comply with R410. Proximity actuated audible signs or other non-visual means within the *public right-of-way* of conveying the information that identifies the alternate *pedestrian access route* shall also be provided.

[R3: Technical Requirements \(access-board.gov\)](#) – R303.2

PROWAG vs. IDOT Standards

On-Street Parking Pavement Markings

MODULE

5

IDOT Standards

- Accessible parking spaces and access aisles shall be designated white pavement markings and may be supplemented with blue pavement markings

PROWAG

- No mention of pavement marking color

41-6.13(c) Pavement Marking

1. Color. Accessible parking spaces and access aisles shall be designated with white pavement marking and may be supplemented with blue pavement markings.
2. Width. Pavement marking shall be 3 in (75 mm) to 6 in (150 mm) wide.

[Microsoft Word - Document2 \(illinois.gov\)](#) – Chapter 41-6.13(c)



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Layout of PROWAG



Chapters of PROWAG

- Chapter 1 – Application and Administration
- Chapter 2 – Scoping Requirements
- Chapter 3 – Technical Requirements
- Chapter 4 – Supplemental Technical Requirements

Layout of PROWAG

Chapter 1 – Application and Administration

- Chapter 1 – Application and Administration
 - Establishes the overarching scope of the document
 - Coordination with other ADA regulations
 - Impact on existing pedestrian facilities
 - Deviations from these guidelines
 - Conventions
 - Definitions

Layout of PROWAG

Chapter 1 – Purpose

- R101 Purpose and Application
 - Scoping and technical requirements ensures pedestrian facilities are located in the public right-of-way and are readily accessible
 - Includes boundary of a site
 - Lies within a site bounded by a property line



Layout of PROWAG

Chapter 1 – Conventions

MODULE

5

- R103 Conventions
 - All dimensions are subject to conventional industry tolerances
 - **Except ranges with specific minimum or maximum endpoints**
 - Calculations of ratios or percentages, round up to the next whole number



[Washington State Dept of Transportation | Flickr.](#)

Layout of PROWAG

Chapter 1 – Important Definitions

- Accessible
 - A pedestrian facility or elements in the public right-of-way that complies
- Building
 - Any structure used or intended for supporting or sheltering any use

Layout of PROWAG

Chapter 1 – Important Definitions (cont'd)

- Pedestrian Facility
 - A structure, route, or space for pedestrian circulation
- Public Right-of-Way
 - Public land acquired for or dedicated to transportation purposes

Layout of PROWAG

Chapter 1 – Alterations

- Alteration
 - A change to or addition of a pedestrian facility in existing and developed public right-of-way that affects pedestrian access
- Developed
 - Containing buildings, pedestrian facilities, roadways, or utilities

Layout of PROWAG

Chapter 1 – Curb

- Curb
 - A raised feature along the side of the street that delineates the edge of the roadway or pedestrian circulation path
- Curb line
 - A line at the face of the curb that marks the transition between the curb and the street

Layout of PROWAG

Chapter 2 – Scoping Requirements

- Chapter 2 – Scoping Requirements
 - Establishes that all newly constructed or altered portions of existing facilities shall comply
 - Provides user with applicable technical requirements based on the scope of work
- **Review Chapter 2 prior to evaluating technical requirements in Chapters 3 and 4**

Layout of PROWAG

Chapter 2 – Alterations

- Transitional segments may be used to connect to existing facilities (R202.2)
- When existing physical constraints make compliance infeasible, compliance is required to achieve the maximum extent feasible (R202.3)
 - Document everything and get sign-offs
- Alterations cannot reduce access (R204.4)

Layout of PROWAG

Chapter 2 – Qualified Historic Facilities

MODULE

5

- When guidelines threaten or destroy historical significance, compliance is required to achieve the maximum extent feasible (R202.5)
- This is determined by State Historic Preservation Officer, or Advisory Council on Historic Preservation (R202.5)



[Google Earth](#)

Layout of PROWAG

Chapter 3 – Technical Requirements

- Chapter 3 – Technical Requirements
 - These technical requirements apply where required by Chapter 2 of the PROWAG

Layout of PROWAG

Chapter 3 – Topics Covered

- Chapter 3 – Technical Requirements
 - **Pedestrian Access Routes (R302)**
 - **Alternative Pedestrian Access Routes (R303)**
 - Curb Ramps and Blended Transitions (R304)
 - Detectable Warning Surfaces (R305)
 - Crosswalks (R306)
 - Pedestrian Push Buttons and Passive Pedestrian Detection (R307)

Layout of PROWAG

Chapter 3 – Topics Covered (cont'd)

- Chapter 3 – Technical Requirements
 - Accessible Pedestrian Signal Walk Indications (R308)
 - **Transit Stops and Transit Shelters (R309)**
 - **On-Street Parking Spaces (R310)**
 - **Passenger Loading Zones (R311)**

Layout of PROWAG

Chapter 4 – Supplemental Requirements

- Chapter 4 – Supplemental Technical Requirements
 - These technical requirements apply where required by Chapter 2 of the PROWAG

Layout of PROWAG

Chapter 4 – Topics Covered (cont'd)

- Chapter 4 – Supplemental Technical Requirements
 - **Protruding Objects and Vertical Clearance (R402)**
 - Operable Parts (R403)
 - Clear Spaces (R404)
 - Knee and Toe Clearance (R405)
 - Reach Range (R406)
 - Ramps (R407)

Layout of PROWAG

Chapter 4 – Topics Covered

- Chapter 4 – Supplemental Technical Requirements
 - Stairs (R408)
 - Handrails(R409)
 - **Visual Characters on Signs (R410)**
 - **International Symbol of Accessibility(R411)**

Questions ?



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15-Minute Break #2





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PROWAG Pedestrian Access Routes

R302: Pedestrian Access Routes



PROWAG – R302

Important Definitions

- Pedestrian Circulation Path
 - A prepared exterior or interior surface provide for pedestrian use in the public right-of-way
- Pedestrian Access Route
 - An accessible, continuous, and unobstructed path of travel for use by pedestrian with disabilities within the pedestrian circulation path

PROWAG – Chapter 2

Pedestrian Access Routes

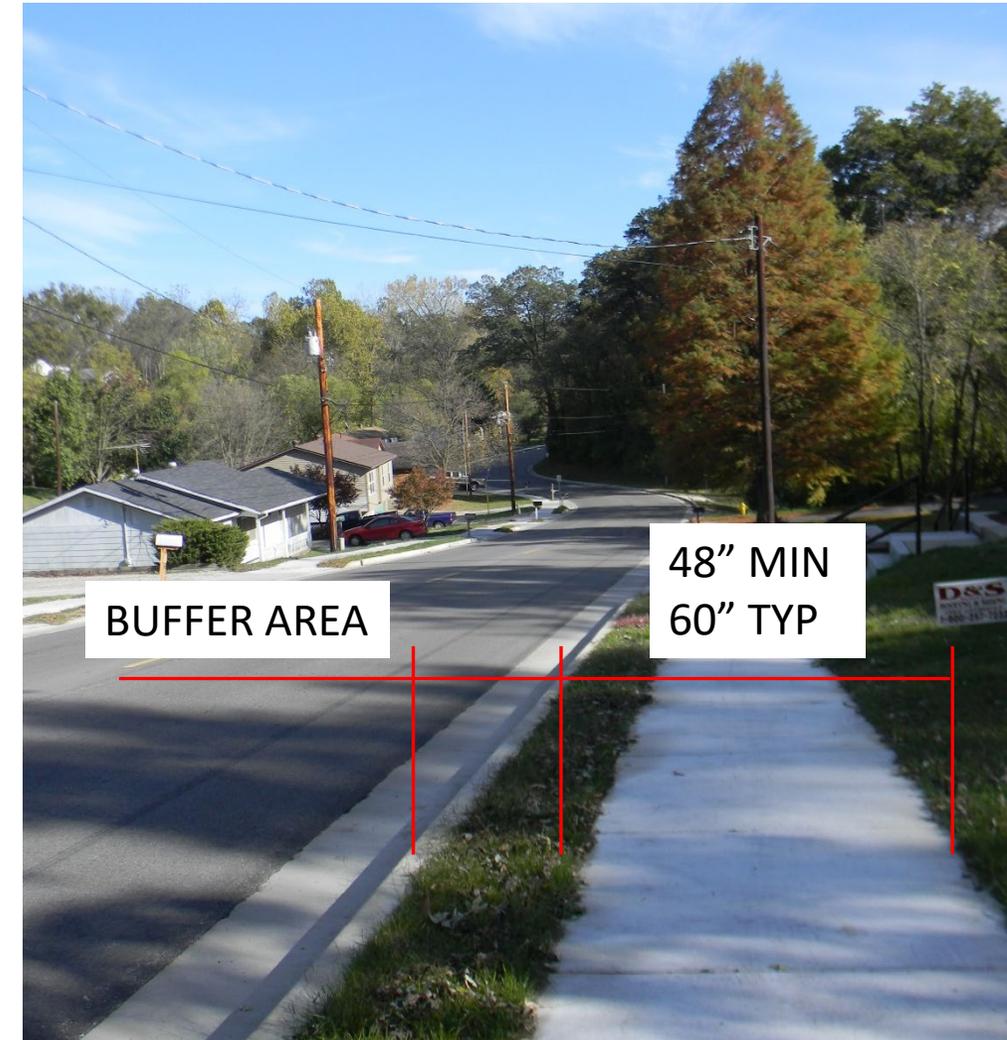
- Pedestrian access routes shall connect building and facility entrances to public sidewalks (R203.2.1)

Exception: Where elements are altered adjacent to the pedestrian circulation path, the path does not need to be altered

PROWAG – R302

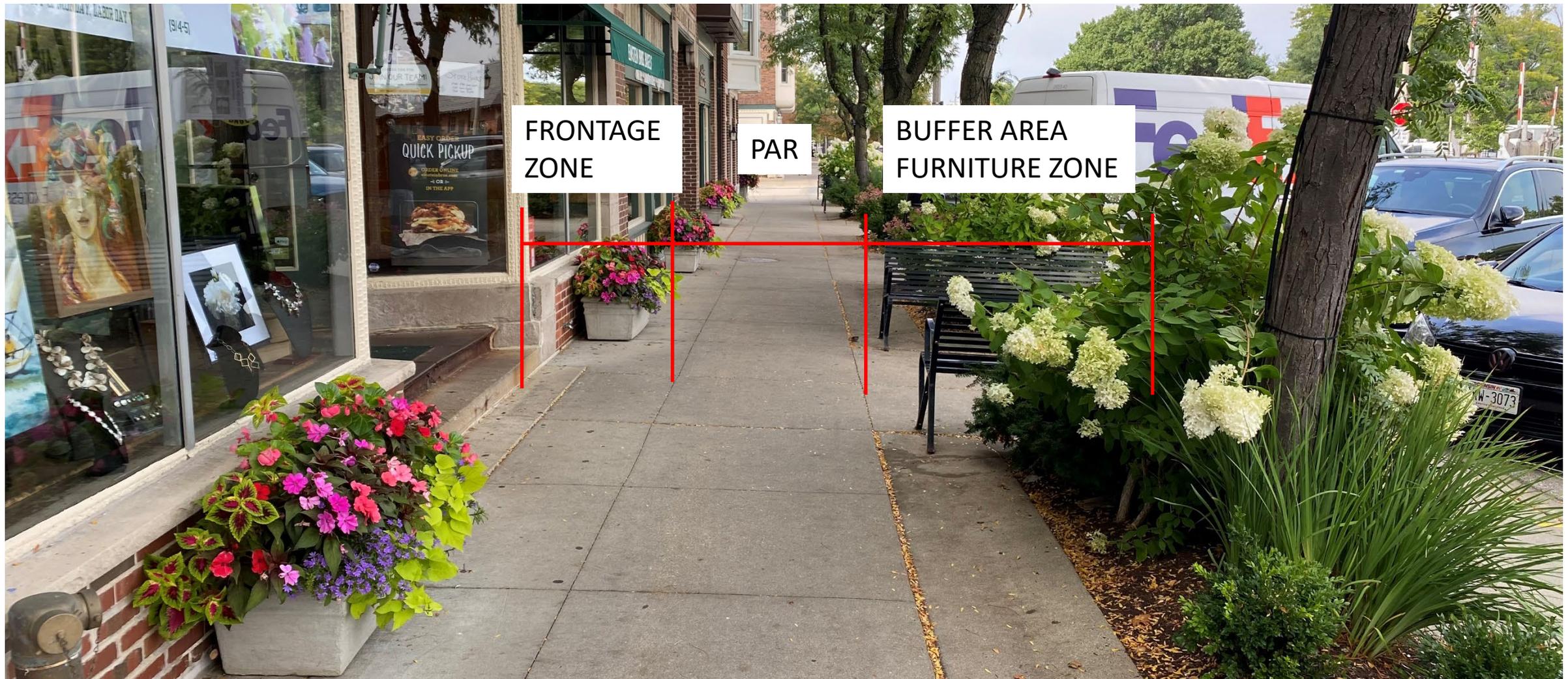
Pedestrian Access Route

- 48" minimum clear width, exclusive of any curb (R302.2)
 - IDOT BLR Manual 31-2.02
 - 60" typical with 2-foot to 3-foot buffer area
 - 72" typical with no buffer area
- 60" by 60" passing space every 200 feet
 - Spaces are permitted to overlap



PROWAG – R302

Sidewalk Zones



PROWAG – R302

Sidewalk Priorities

MODULE

5



PROWAG – R302

Running Slope

- Within the highways right-of-way
 - Running slope of the sidewalk shall not exceed the grade of the adjacent street (R302.4.1)
- Outside highway right-of-way
 - Running slope shall not exceed 1:20 (5%) (R302.4.2)



PROWAG – R302

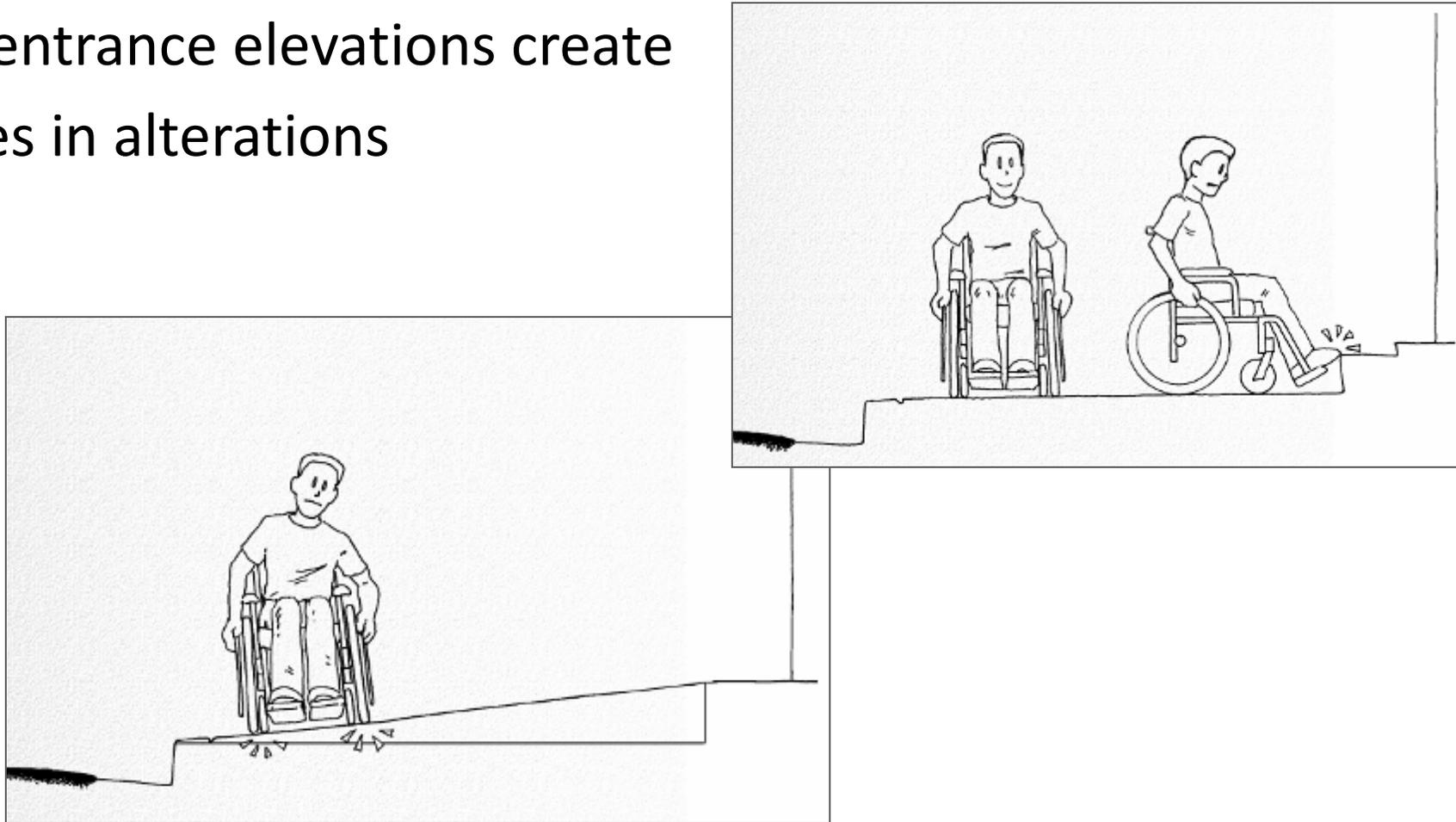
Cross Slope

- 0% is best for wheelchair users
- Partial slope is needed for drainage
- 1:48 (2.1%) maximum (R302.5.1)
 - Suggest 1.5% cross slope is to stay within 2.1% maximum
- “Level” means 2.1% maximum slope

PROWAG – R302

Cross Slope Challenges in Alterations

- Building entrance elevations create challenges in alterations



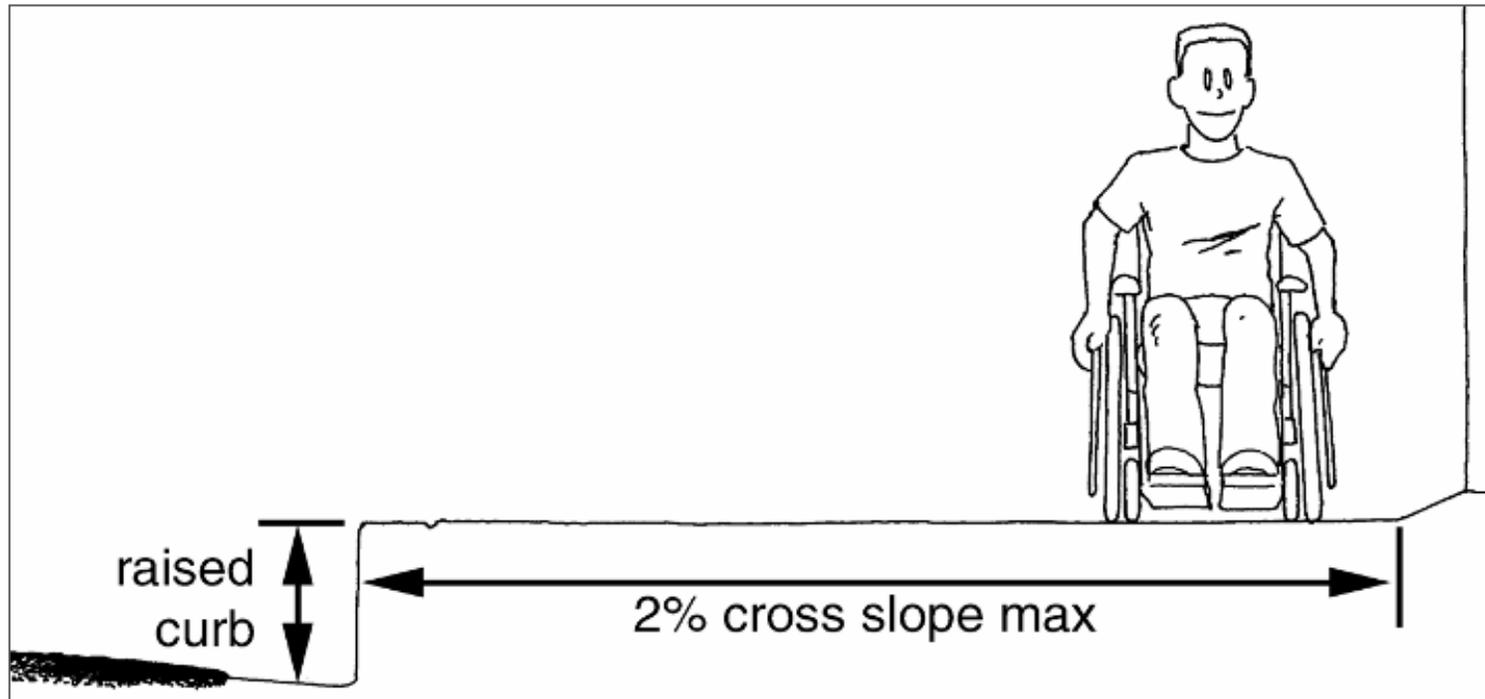
PROWAG – R302

Cross Slope Challenges – Potential Solutions

MODULE

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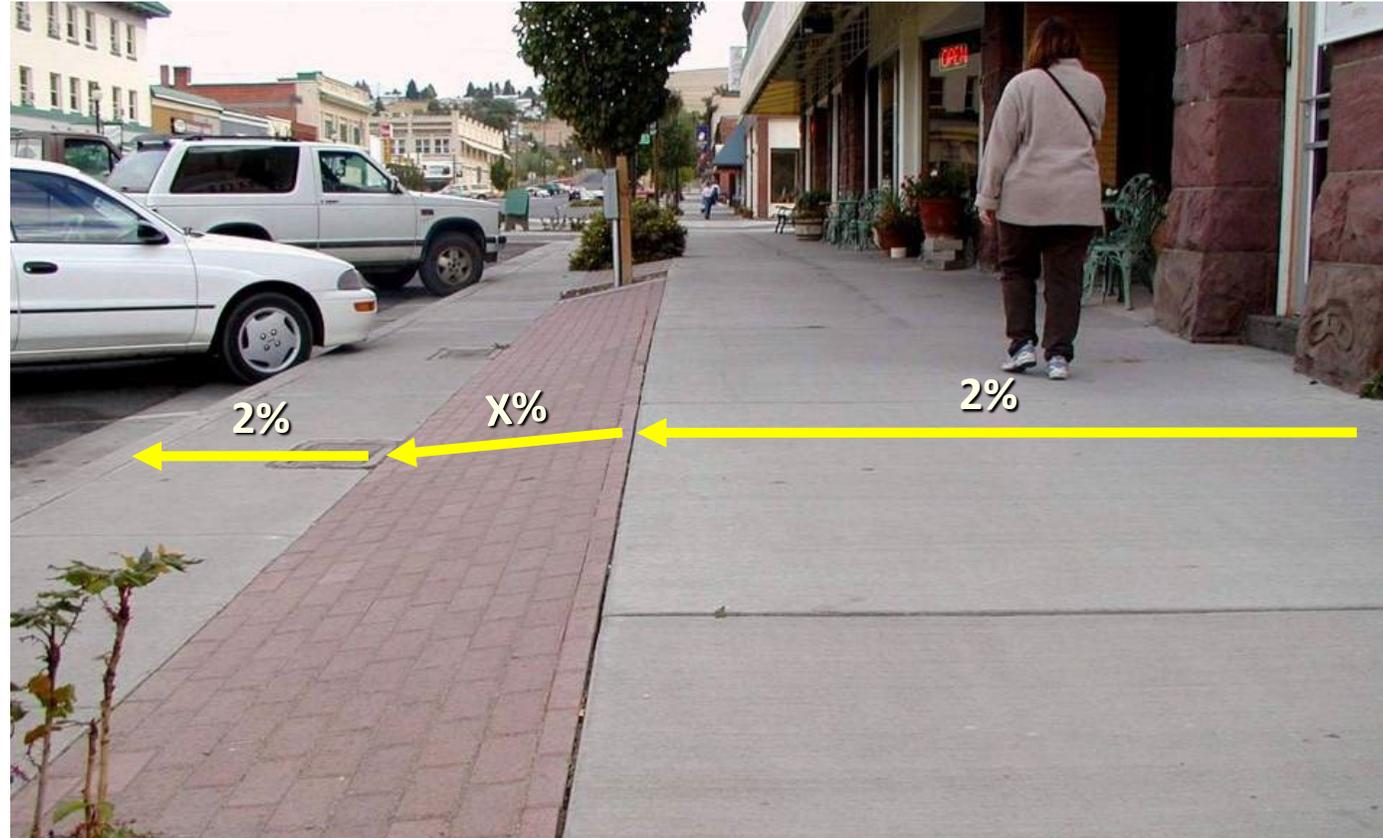
- Raise curb



PROWAG – R302

Cross Slope Challenges – Tabled Solution

- Tabled solution



PROWAG – R302

Surfaces

- Walking surface shall be stable, firm and slip resistant (R302.6)
- PROWAG does not specify a specific material
- Pervious pavement



PROWAG – R302

Surfaces – Brick and Paving Stones

MODULE

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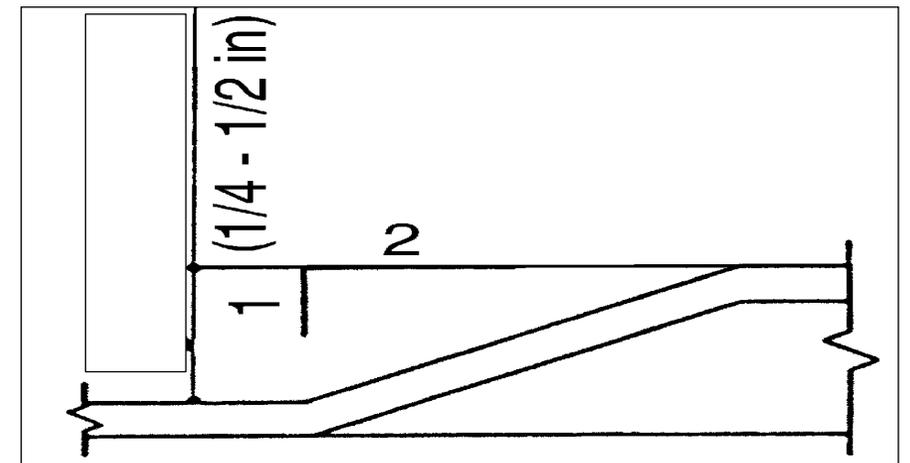
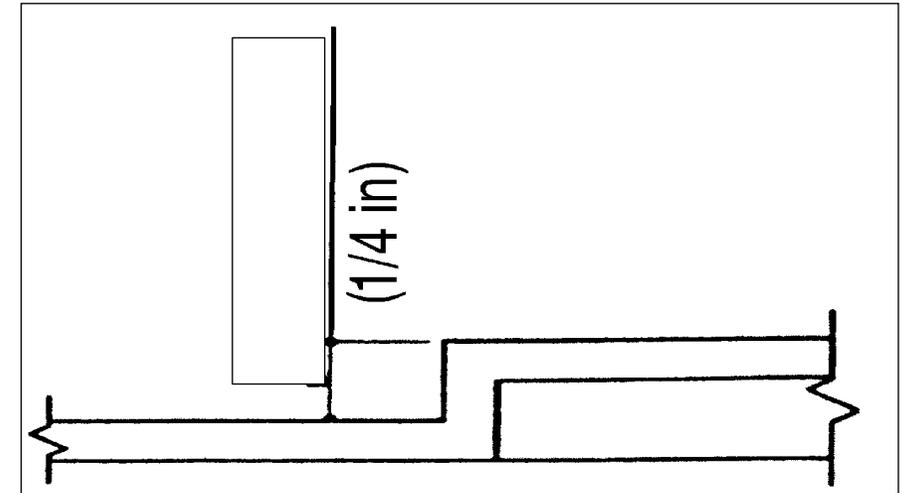
- Properly installed and well maintained
- Narrow and flush grout lines
- Challenges:
 - Sidewalk base
 - Material selection
 - Installation
 - Maintenance



PROWAG – R302

Changes in Level

- Maximum $\frac{1}{4}$ " vertical
- Changes in level between $\frac{1}{4}$ " and $\frac{1}{2}$ " shall be beveled not steeper than 1:2
- $>1/2$ " change in level requires a ramp (R302.6.2)





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PROWAG Alternate Routes

R303: Alternate Pedestrian Access
Routes



PROWAG – Chapter 2

Alternate Pedestrian Access Routes

- When a pedestrian circulation path is temporarily not accessible due to construction, an alternative pedestrian access route **must** be provided (R204.1)

EXCEPTION: If establishing the route is technically infeasible, an alternate means of providing access shall be permitted

- Where accessible transit stops are temporarily not accessible due to construction, alternate transit stops shall be provided (R204.2)

- Signage
 - Shall be provided in advance of decision points and shall comply with R410 (R303.2)
- Surface
 - Shall not be less accessible than the surface of the closed path (R303.3)
 - Grass and dirt are not considered acceptable

- Clear Width
 - Minimum clear width shall be 48 inches, exclusive of curb width (R303.4)

EXCEPTION: When the temporary route uses an existing sidewalk, the width of the sidewalk shall not be less than the temporarily closed sidewalk

- Curb Ramp or Blended Transition
 - Where the alternate route crosses a curb, a curb ramp or blended transition complying with R304 shall be provided (R305.5)



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PROWAG On-Street Parking

R310: On-Street Parking Spaces

R311: Passenger Loading Zone



- Passenger Loading Zone
 - An area specifically designed or designated for loading and unloading passengers, but does not primarily serve vehicles on a fixed or schedule route

PROWAG – Chapter 1

Important Definitions – Block Perimeter

- Block Perimeter
 - The near side of the streets surrounding a block
 - Southside of Duane Street
 - Westside of Forest Avenue
 - Northside of Hillside Avenue
 - Eastside of Main Street



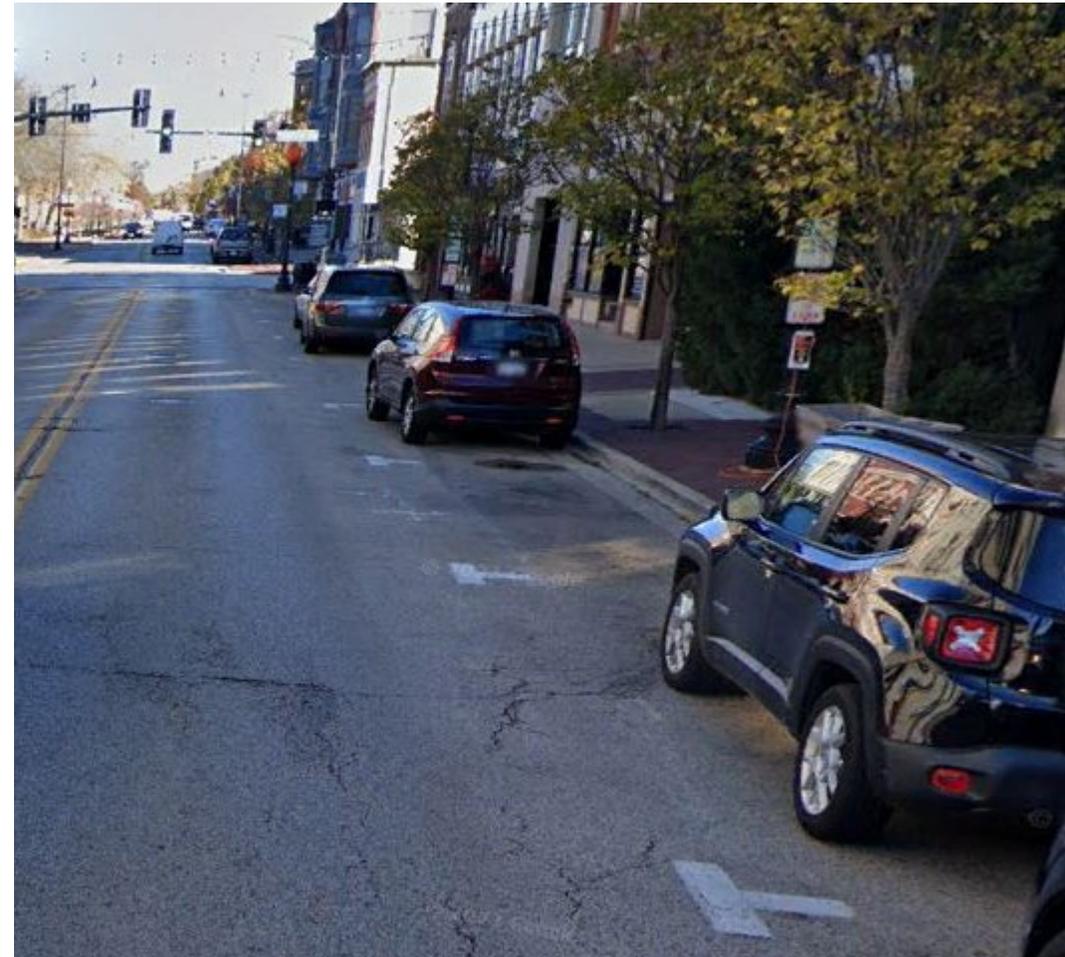
PROWAG – Chapter 2

On-Street Parking - Required Spaces

MODULE

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- Where on-street parking is provided and metered / designated by sign, accessible parking spaces are required (R211.1)
- If spaces are not marked, every 20 feet of designated parking is counted (R211.2)
- Fortytune vs. City of Lomita



PROWAG – Chapter 2

Table R211 On-Street Parking Spaces

Total Number of Metered or Designated Parking Spaces	Minimum Required Number of Accessible Parking Spaces
1 to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
101 to 150	5
151 to 200	6
201 and over	4 percent of total

[R2: Scoping Requirements \(access-board.gov\)](#) – Table R211 On-Street Parking Spaces

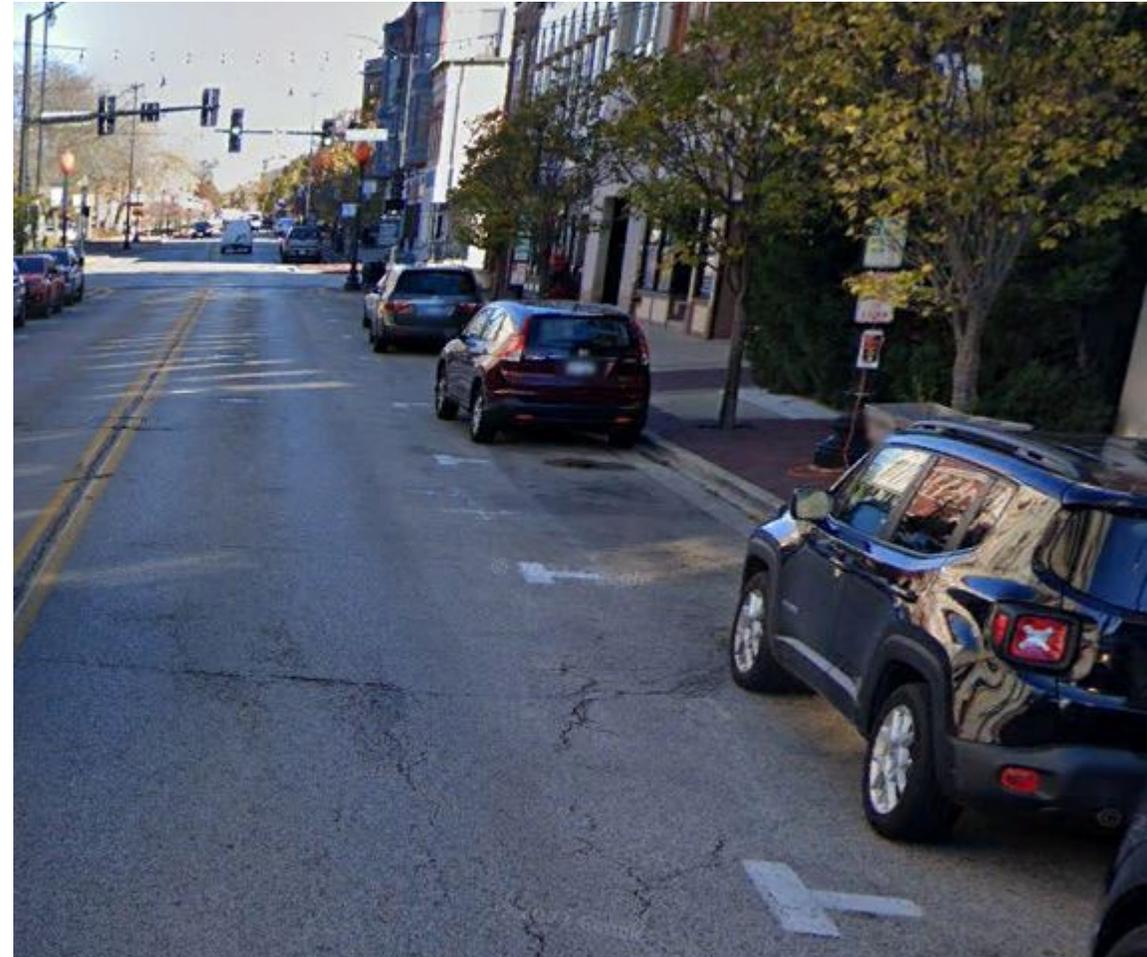
PROWAG – Chapter 2

On-Street Parking – Connection to Sidewalk

MODULE

5

- Accessible parallel parking spaces
 - Curb ramp provided at either end of the parking space (R203.6.1.3)
- Perpendicular or angled parking
 - Curb ramp provided to connect access aisle to pedestrian access route (R203.6.1.4)



[Google Earth](#)

PROWAG – R310

Parallel Parking Spaces

- Parallel on-street parking spaces
 - Shall be 24 feet long and 13 feet wide minimum (R310.2.1)
 - Parallel parking cannot encroach on the traveled way

EXCEPTION: Where parallel parking spaces are altered but the adjacent sidewalk is not, the spaces can match the dimension of the adjacent spaces, if provided nearest the crosswalk and a curb ramp is provided

In alterations, providing the dimensions shown above would result in an available right-of-way less or equal to 9 feet, measured from the curb line to the right-of-way line

PROWAG – R310

Parallel Parking Spaces - Example



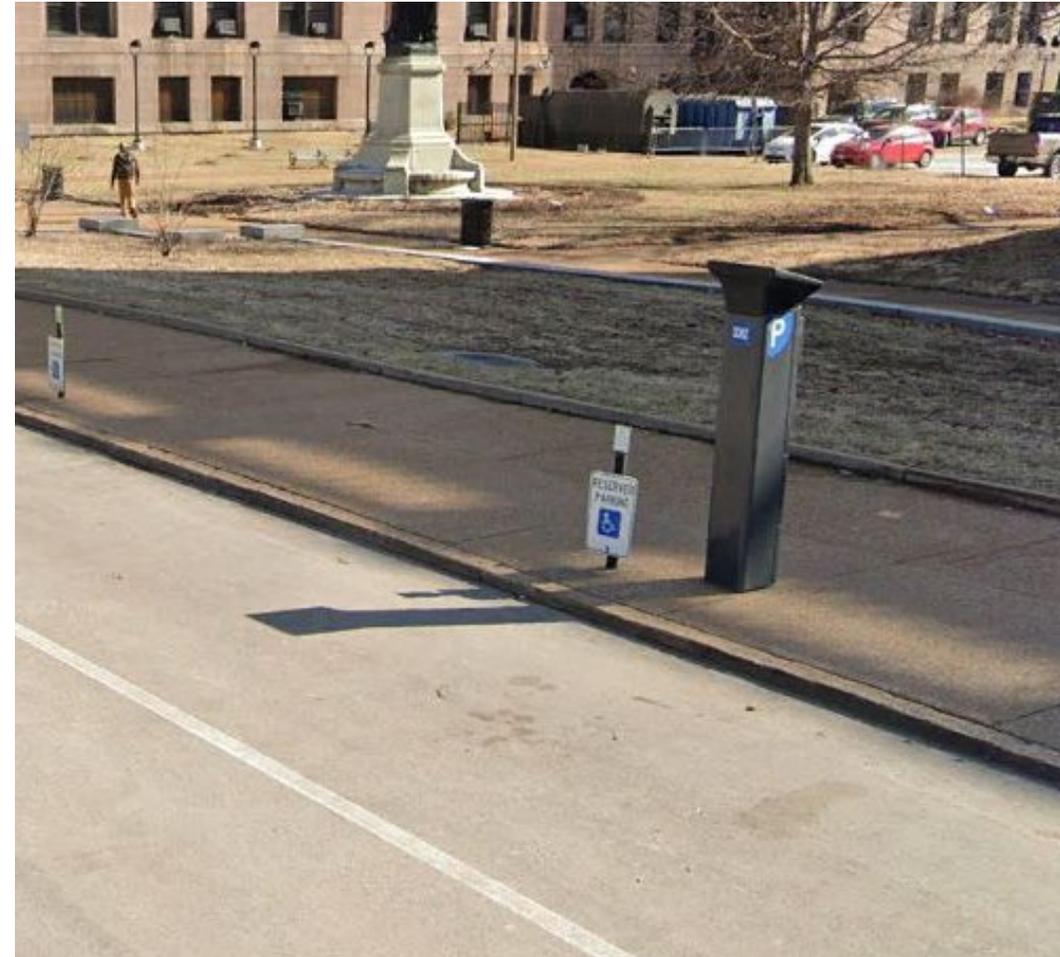
PROWAG – R310

Parallel Parking Spaces - Standards

MODULE

5

- Center 50% of the length of the sidewalk adjacent to an accessible parking space shall be free of obstructions (R310.2.4)
- Parking space shall be identified by signage per R411 (R310.2.5)
 - Mounted 60-inch minimum above the ground surface



[Google Earth](#)

PROWAG – R310

Perpendicular Parking Spaces

- 96-inch-wide access aisle shall extend the full length of the parking space (R310.3.1)
- Where the access aisle serves only one parking space, the access aisle shall be located on the passenger side of the vehicle (R310.3.1)



PROWAG – R310

Angled Parking Spaces

- Width of the angled parking space shall be 132 inches (R310.4.1)
- Adjacent access aisle shall be 60 inches, extending the full length of the parking space on the passenger side (R310.4.2)



PROWAG – R311

Passenger Loading Zone

- Vehicle pull-up space shall be 96 inches wide by 20 feet long (R311.2)
- Adjacent access aisles shall be 60 inches wide and extend the full length of the vehicle pull-up space (R311.3)
- Center 50% of the length of the sidewalk adjacent to an accessible parking space shall be free of obstructions (R311.3.1)
- Access aisle shall be marked (R311.3.2)



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Design Standards Resources

Resources



Design Standards Resources

Local Resources

Chicago Metropolitan Agency for Planning (CMAP)

- [Accessibility and ADA - CMAP \(illinois.gov\)](https://www.illinois.gov/transportation/transportation-planning/transportation-planning-2014-2016/transportation-planning-2014-2016-ada)

Design Standards Resources

Federal Standards

MODULE

5

Americans with Disabilities Act (ADA)

- Establishes scoping and technical requirements for Title II entities
- [ADA Title II Regulations](#)

Americans with Disabilities Act Accessibility Guidelines (2010 ADA)

- Updated accessibility standards issued under the ADA
- [2010 ADA Standards](#)

Public Rights-of-Way Accessibility Guidelines (PROWAG)

- Guidelines for pedestrian elements on public rights-of-way
- [PROWAG Guidelines](#)

Design Standards Resources

State Standards

[Illinois Accessibility Code](#)

- Supports ADA Title II compliance

[Illinois Department of Transportation \(IDOT\)](#)

- Highway and District standards

[Great Lakes ADA Center](#)

- Technical assistance and training



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Presentation Goals and Future Sessions



Review of Presentation Goals

- Understanding the history of accessibility rule-making
- Application of federal design standards
- Comparing federal vs. state vs. local code
- Application and Scoping of PROWAG
 - Application (R1)
 - Scoping Requirements (R2)



Review of Presentation Goals (cont'd)

- Technical requirements of PROWAG
 - Pedestrian Access Routes (R302)
 - Transit Stops and Shelters (R309)
 - Protruding Objects (R402)
 - Signage (R410 & 411)
 - On-Street Parking Spaces (R310)
- Curb ramps and pedestrian signals will be covered on Day 2



Short Follow-up Survey



Next Steps

- What do we do with all this information?
- What needs to be fixed first?
- How do I sell the value of increasing accessibility?

Join us tomorrow!





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Future Training Sessions in Spring 2024

Tomorrow, March 6th, at South Elgin
Public Works – 9am to 1 pm

[Future trainings \(cmap.illinois.gov\)](https://cmap.illinois.gov)



Final Questions?



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Thank you!

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